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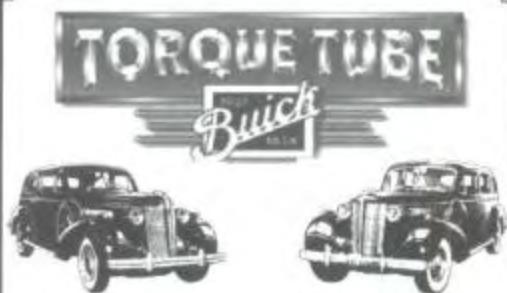
VOLUME XXIV - NUMBER 5 - MAY/JUNE 2006

1937 *BUICK* 1938

TORQUE TUBE

THE INTERNATIONAL NEWS PUBLICATION
FOR MEMBERS OF THE 1937-1938 BUICK CLUB





Founded in 1980, the
TORQUE TUBE

is published every two months
for the education and enjoyment of the
1937-1938 BUICK CLUB.

World-wide membership dues
are \$40.00 per year.

Membership year is
September 1 to August 31
of the following year.

Persons joining during the year
receive all back issues
from September 1.

All payments must be in U.S. funds.
Please make your check payable to
1937-1938 BUICK CLUB.

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1937-1938 BUICK CLUB website:
www.torquetube.com

Please send all mail to:

TORQUE TUBE
P.O. BOX 21000
OAKLAND, CA 94620
USA

You can reach editor Mark Jordan at:
415-203-4180
torquetube3738@yahoo.com

Editor: Mark Jordan (#1297)
Asst. Editor: Giancarlo Davis (#1748)
Copyeditor: Thomas Burchfield (#1776)
Design & Layout: Tanoa Stewart (#1823)

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9TH CYLINDER



Jose Pardo (#558) sent us in this image of four 1937 Buicks at the National Transportation Museum in Columbia, South America. They have '37 Roadmaster, two sedans, and a Special Coupe! WOW! You can check out the museum online at: <http://www.museodetransporte.org/autos.htm>, but be sure to brush up on your Spanish!

David Galinas (#1078), from New Hampshire, sent in this great old photo that he bought on eBay. The original photograph was acquired from the collection of Floyd Clymer, Los Angeles, California.

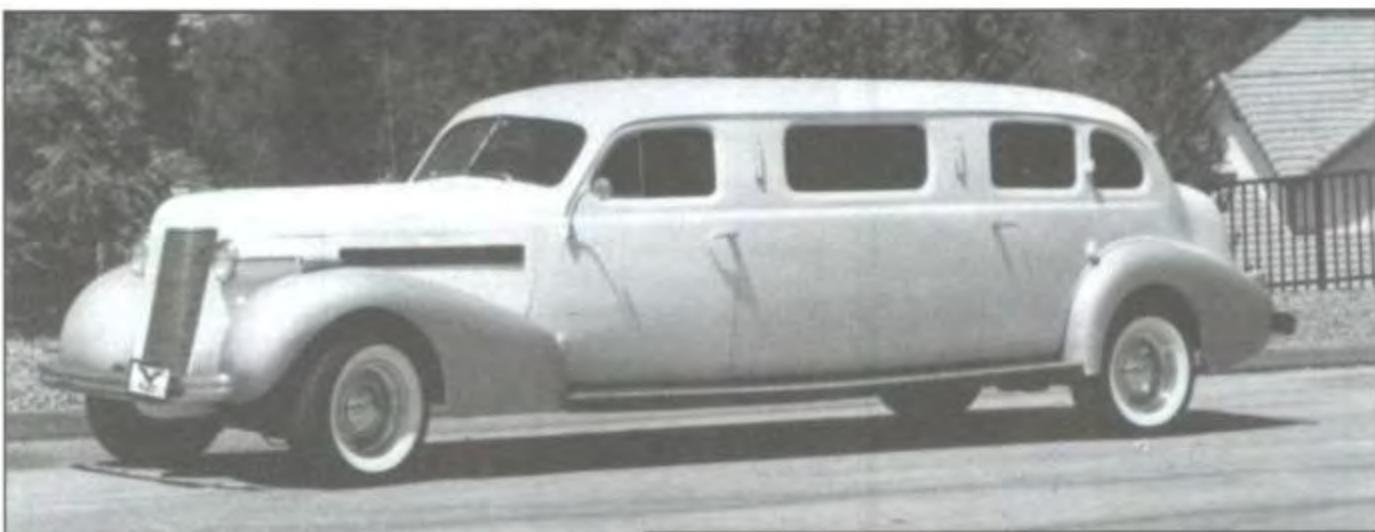


IMPORTANT CLUB NEWS!!!

I have decided to continue with the editorship for one more year, although it means making the Torque Tube a quarterly publication, i.e., it would appear four times a year. To make up for this cutback, I would increase the page count. Due to time restraints, this would be the only way to keep the club going. Dues would stay the same because of the increase in pages. Please let me know if this is acceptable to you as members. I figured having the club continue is better than letting it die. If you have any concerns or comments, please feel free to contact me.

Good news! Torque Tube has won the 2005 Golden Quill Award from Old Cars Weekly Magazine. I would like to thank all our members that have contributed articles and information that helps us publish the Torque Tube. Please keep up the good work; we always need our members' support. Winning the award is not that easy. Some of the criteria include: 1) We are balanced and offer club members the necessities for the hobby; 2) We make a respectable appearance; 3) Time and effort is reflected in the pages; 4) High standards in accuracy and in story selection.

Soon to be new member David Tribble, from Sacramento, California, sent in these pictures of his stretched 1937 Buick, that he is now restoring. This will be the car's second major restoration and we hope David has some good luck with it.



Arnold Hiott (#1876) sent in these three pictures of his car collection, including a 1938 41, Desoto, and Cadillac that he is currently restoring. Thanks for the pics (below, right and right/below)!



Ed Johannemann (#1469) wanted me to print this nice letter about JB Donaldson, one of our advertisers:

Dear Fellow 37/38 Members,

I have been doing restorations for about fourteen years and have used many suppliers. Few providers have lived up to their advertisements and promises. I admit that I am a fusspot, as my wife calls me, and very hard to please, but I found a supplier that met all my requirements and more. One of the magazine's advertisers is Donaldson Steering Wheel Restorations. They did the wheel on my 1938/41 and it could not have come out better. The work was impeccable. I had purchased another wheel on line and it arrived cracked at all the "banjo" connections and had a few cracks in the main part of the wheel, so I was leery of getting another wheel with flaws. Donaldson's wheel was perfect. The look and the feel could not be better. Spend a few extra bucks and get it done right! You will not be disappointed.

A hearty thanks to the guys at Donaldson's!!!

Ed Johannemann
Alexandria, VA



Happy Motoring!

Mark

'37-'38 EASTERN MEET BIRMINGHAM, ALABAMA

By Sherry Bargar #1496

Another year, another fantastic Meet! John and Alverta Cover had the week well planned out. We drove a lot and saw a lot and in general had a wonderful time.

My little foursome, in my 1999 Dodge Van (which looks nothing like a Buick of any kind) left Marion, Ohio, bright and early on April 30. Bob and Archie Holliday and my friend Shirley Baldauf and I traveled south through some rain and eventually, after some hits and misses, met up with a group of other Club members from Ohio, Michigan, and Indiana who were traveling south. We spent Sunday night in Lebanon, Tennessee. We had an

early start again on Monday morning and, like most of the others, arrived in Birmingham, Alabama, in good time on Monday afternoon.

We checked into our rooms and then checked with John for our Meet packets. John and Alverta worked hard on these packets. We were given a spiral bound book with all, and I mean all, pertinent information. There were maps, mileage, information about our destinations, history of the area, and other stuff I am still reading about. Besides the maps and mileage, one of the most important things John included was King's Rules of the Road (as in Steve and Pat King!). Steve and Pat have put a lot of these road trips together and they have some really good rules to follow.



The Buick group in front of the Barber Museum

Brianna in the rumble-seat of Carl and Dorothy Anderson's 1937 convertible Coupe. Doesn't she look natural there!?



Our motel was located on the side of a hill in Birmingham. Some of us had rooms up the hill; some had rooms down the hill. Most of us did not even try walking up or down! (I did take the stairs once, but wow what a staircase! I didn't do that again!) It made for beautiful surroundings though. After we were checked in and rested a little bit, we all headed to Dale's Southern Grille for supper. It was good food and good companions for our first night in the big city.

On Tuesday morning, John split up the group, Charles Dowden of the local Buick Chapter led one group, while John took the other group. These smaller groups made it easier to stay together and, since each group communicated on separate CB channels, we were able to hear the directions and instructions better. At least that was the theory. For the most part it worked.

Shirley and I rode with Marv and Phyllis Rhynard, in their '38 while Bob and Archie rode with Pat and Steve King in their '37. There is an advantage of not actually owning a '37-'38 or not driving it to a Meet—you get to ride with different people in different cars. That is, if they don't run screaming when they see you headed their way. If they do that, you know you were not a good traveling companion the last time you rode with them.

Our first stop was the Southern Museum of Flight, we were given a very interesting tour and got to see many different types of airplanes. One plane was a B-25 from 1942 that had recently been recovered from the bottom of a lake in the Carolinas.

The Whistle Stop Café was our lunch destination, a quaint little restaurant that was the inspiration for the movie *Fried Green Tomatoes*. Fannie Flagg, a famous actress and writer, is the great

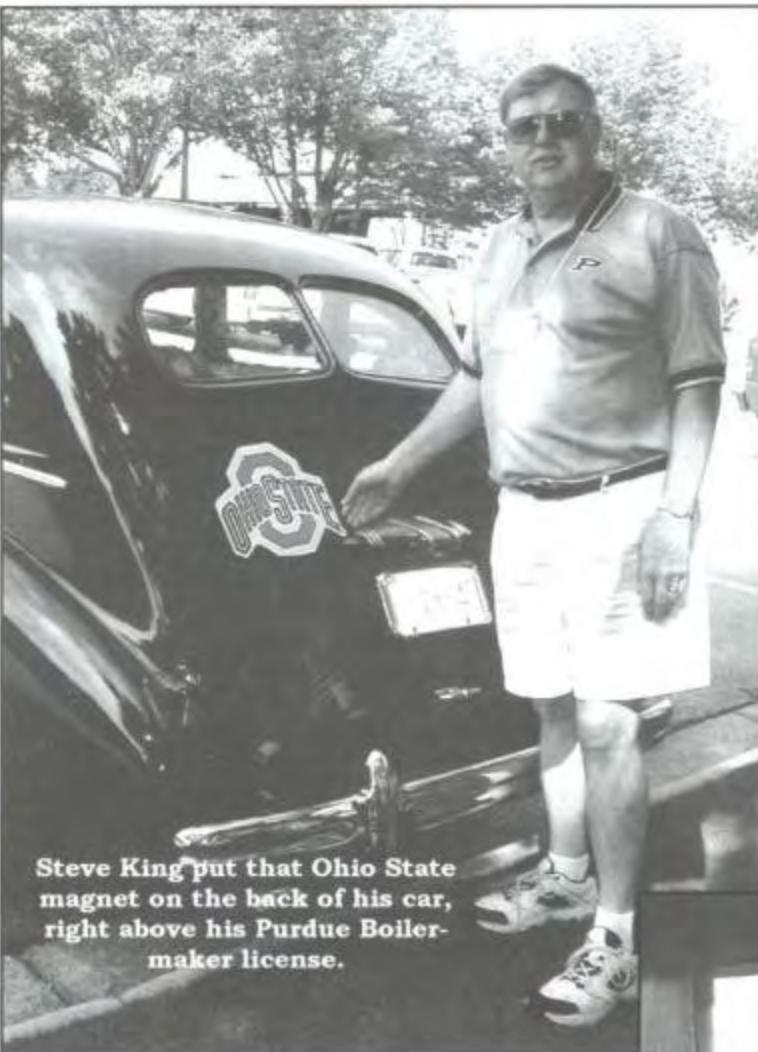
niece of Miss Bess Fortenberry, the restaurant's one-time owner. Ms. Flagg spent a great deal of time in the café and later used her memories to write a novel, which was made into the movie. The food was true southern cooking and of course the friend green tomatoes were great.

We were in for a treat when Mr. Vic Zannis stepped up to tell us about the 1997 Peking-to-Paris Road Rally. Mr. Zannis helped prepare two 1950 Fords for the race and was mechanic and navigator for one of the cars that placed First in Class and Second Overall in the rally.

From the Whistle Stop Café we traveled to Barber's Museum, one of the largest motorcycle museums in the country. Now let me tell you this was some museum! The guys actually drooled! It was a beautiful building. It also had the world's largest collection of Lotus cars. There were some other beauties



Steve King presenting the Popular Perennial Putter to Dave Wettersten.



Steve King put that Ohio State magnet on the back of his car, right above his Purdue Boiler-maker license.

as well, some old and some new. (No, I don't mean all of us—I mean the cars!) The place was spotless and the motorcycles were displayed on racks one over the other clear to the ceiling and I'm not talking 8-foot ceilings! It was very impressive, even to a woman!

After we had our picture taken with some of the Barber people, we had a delightful Southern-style barbecue dinner at Aldridge Gardens, next to the lake. It was casual and relaxing and everyone enjoyed it. However, while Shirley and I took a walk around the lake, as others had done, the most of the rest of the bunch got in their cars and left us. Fortunately, Marv and Phyllis decided to wait for us. (We thought we had done something to offend someone, maybe like putting an Ohio State University magnet on the back of a Purdue Boiler Maker, which is Steve King's car. Steve and I have a friendly rivalry there!)

By the time we got back to the motel that evening, we had driven approximately fifty-one miles through some beautiful Alabama

countryside and had been gone all day. No naps for us!

Wednesday morning began even earlier than Tuesday. For someone who is not a morning person it is quite a struggle to be up and beautiful and ready to head out at 7:45 a.m. (Well, I managed the "up" part!) Today, Shirley and I rode with the Kings, while the Hollidays rode with the Rhynards. We like to spread ourselves around so everyone knows they only have to put up with us for a day or two.

We got to see some more of the lovely Alabama scenery. Who knew Alabama was so hilly!?

The highlight today was the Jack Warner Art Museum. Now I must confess my first thought was "Oh no, not an art museum" but what a wonderful art museum! We were fortunate enough to have Mr. Warner, who is ninety-one, speak to the group. Mr. Warner's collection illustrates American History through art, so it wasn't abstract art and the like. It was all very interesting and explained so clearly. Mr. Warner even invited us to his home and garden. That was a rare treat. His garden was vivid and unusual and we were able to spend a great deal of time there soak-



Shirley Baldauf and Sherry Bargar dressed as 30's gangsters.

The lineup, 1937—1938 Buicks.



ing up its beauty and relaxing among the blooms overlooking the lake. Mr. Warner was a gracious host, providing us with lemonade and cookies.

Before we went to Mr. Warner's home, we had lunch at Cypress Inn. What a nice meal with a beautiful view of the water!

We were to take a tour of the historical buildings at Tannehill State Park, but we enjoyed ourselves so much at Mr. Warner's home, we ran late. We arrived in time to have a supper of typical southern fare at the Furnace Masters Restaurant. We rested on the front porch, singing a few old-time songs and waiting for Birmingham's rush hour traffic to thin.

We were scheduled to have our annual

miniature golf tournament after supper, but it was late when we got back to the motel and we were all tired from our wonderful day of sightseeing. So, we decided to do a "Hole-In-One" at the banquet.

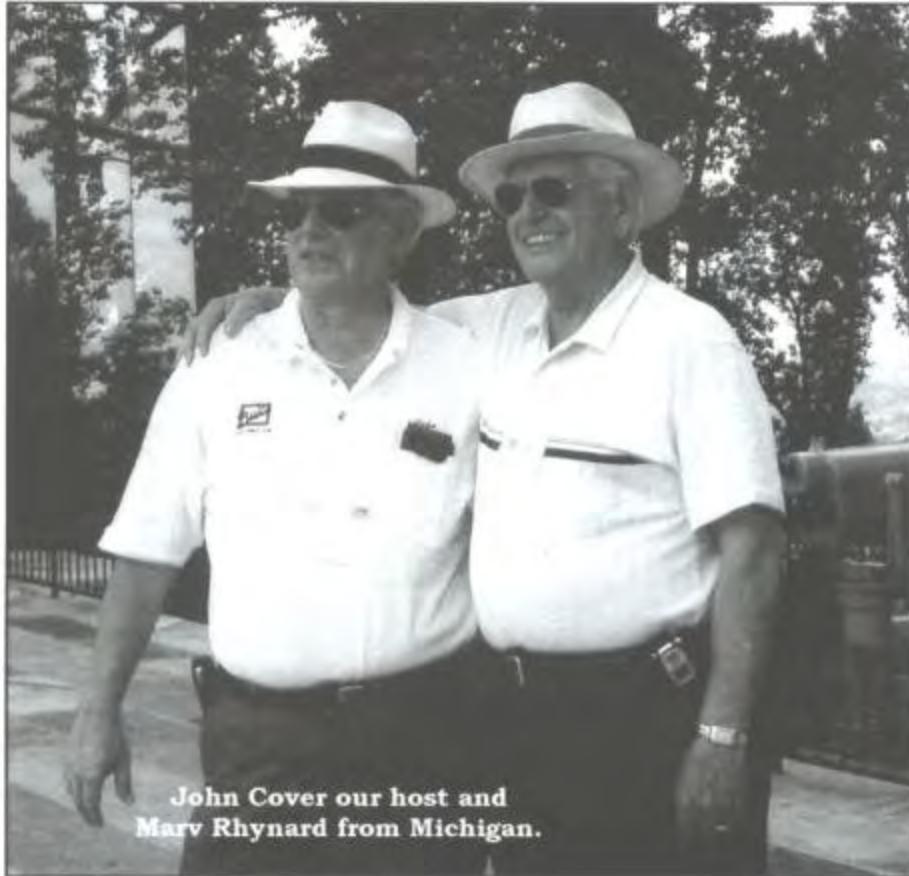
Thursday morning began bright and early. We traveled again through some beautiful Alabama scenery to the Botanical Gardens. The ladies were dropped off to enjoy the gardens while the guys went to Hesco. The ladies had a relaxing time and enjoyed strolling the gardens. Jane Hinds gave a brief but informative talk on roses.

Apparently, the guys had a wonderful time at Hesco. Some of them came back a little starry eyed. Hesco owner Lee Hurley has a nationally syndicated radio program on automotive mechanics. After a tour of the shop, they met us back at the Botanical Gardens for a light lunch at the French Cafe.

After lunch we went to Vulcan. No, this is not the home planet of Mr. Spock of *Star Trek* fame, but the largest cast-iron statue in the world. We took a self-guided tour and an elevator took us to the top of the base of the statue where we had a fantastic view of Birmingham.

We were able to rest a little bit back at the motel before the banquet at The Club. We were asked to dress in period style for dinner. It was fun to see what some came up with. Dan and Linda Lash looked very authentic, as did Marv and Phyllis Rhynard. Shirley and I decided to go as "gangstas," machine guns and all. Everyone looked great, though some looked more authentic than others.

As usual we had entertainment. Not the best maybe, but entertainment nevertheless. Shirley and I read a poem that she had



John Cover our host and
Marv Rhynard from Michigan.

written. Pat and Steve King performed their skit with Beulah (and made some derogatory remarks about Ohio State). Danny Vincens sang his famous Ice Cream Song and the "Straight Eights" sang "Yes Sir, That's My Buick" to the tune of "Yes Sir, That's My Baby." (The words were written by our lyricist Marv Rhynard). We also had our "Hole-In-One" shootout for the amazing golf trophy. Dave Wettersten was this year's big winner. He must now add something of import to the Popular Perennial Putter Prize. All that amazing entertainment made for a fun evening. I think most of us hated to see it end.

My van loaded up early Friday morning and got in line with the '37 and '38s heading toward Ohio, Indiana, and Michigan. We spent one more night with good companions. On Saturday, somewhere near Louisville, Kentucky, we went our separate ways with the hope that we can get together some weekend this fall. We had such a great time at this Meet that we will keep in touch with each other and try to get as many of us together as we can sometime during the year following the Meet. It



Brianna Erza and Jesse Cahue the III sit down for dinner.

doesn't always work, but we try. We are shooting for Shipshewana, Indiana, on October 6 and 7 this year. Hope it all works out. Check with Pat King for details.

Thanks to John and Alberta for all the hard work you put into the Meet. You did a great job. We covered a lot of ground and saw a lot of stuff. It was great to see Brenda and Danny Vincens. They lost their New Orleans home but made it through Katrina and had a lot of horrific stories to tell. We were all greatly saddened by the death of Bob Lawrence just days before the Meet. Our hearts go out to Arlene at this difficult time. We were happy to

see Jesse and Sandy Cahue's grandson Jesse the 3rd again this year. He is growing up fast and what a looker! It was also great seeing Brianna, Carrie, and Dave Wettersten's granddaughter. My, what a lovely young lady she is growing into. They both seem to enjoy their week with the "old folks."

All in all, it was another pleasant week with friends we have grown to know and love. We are looking forward to next year's Meet in Michigan already. Marv and Phyllis Rhynard are in charge and I know they will do a great job.

See you all in '07!



Jane Goddard, Marv Rhynard, Phyllis Rhynard and Gene Phillips dressed for the 30's.

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By: FRANK FREDA, SANTA BARBARA, CA
Phone: (805)-969-7500
Email: frankfreda@cox.net

TORQUE TUBE TECHNICAL INDEX

The technical index, approximately 30 pages, is completed and available to all Buick Club Members for \$5.00 cash or check, prepaid. Any copies of any articles will be available for \$1.00 per article, postage included.

The index was compiled from reading all 37-38 *Torque Tube* club magazines issued from 1982 to July/August 2005. More than 850 technical references are logged. Some are duplicates, due to the different approaches taken by different Buick hobbyists. No effort was made to determine the accuracy of the technical information, neither does the index make me an expert to give advice on 37-38 Buick's.

The purpose of the index is to create a complete *Torque Tube* library of all technical articles written over the years by hobbyists and make them available to all club members and 37-38 Buick enthusiasts. Some articles are only a few lines, some are a few pages, and some are reprints from factory bulletins or service manuals.

The index is formatted in five columns, as follows:

1. File column in my personal numbering system
2. Volume column in roman numerals
3. Number column in issue numbers
4. Page column in page the article starts on
5. Article column in articles listed in alphabetical order

If you are interested in receiving the index and/or any articles, please send your request with cash or check to my home address:
Frank Freda, 345 Ridgecrest Dr., Santa Barbara, CA 93108

Please include with your request for any technical articles my personal file number in the first column to the left.

It was a joy to read all the issues published by the Buick Club and to compile this index.

Frank Freda, member #1838, is the owner of 37 Buick-46C Sedan Blue Convertible, a 48 Buick Super Black Convertible and is seriously looking for a nice 37-38 Century Sedan.





TECHNICAL TIPS

By Jon B Kanas (#1732)

Fender Light Structural Integrity – An Approach for Preservation

Installation of the newly reconditioned fender lights was one of the last items of reassembly required during the restoration of my 1937 66C. Significant and unexpected damage took place during the installation; specifically, the base of the light disintegrated upon minimal tightening



of the light to the fender. At the point in the picture, the base had broken, yet the light was still relatively loose on the fender. This light base, when prepared for painting, was stripped and thoroughly inspected. We did not find any cracks or significant corrosion.

My assessment determined that there were several contributing factors leading to this disaster. First, the fender light base was made of a type of pot metal which had become extremely brittle with age. Second, the edges of the light made contact with the surface of the fender before the threaded studs, putting significant outward pressure on the sides of the fender light base as the studs were pulled down into the fender.

I have developed a simple and inexpensive solution that I believe effectively prevents the outward pressure on the base of the light.

First, fill the base of the light with epoxy in such a way that the epoxy is slightly above the edges of the light when cured. This removes the pressure on the sides of the base, by providing a much larger surface area over which the stress is distributed, and the sides are now bonded to the epoxy core, improving their structural integrity.

Clean the entire inside surface of the fender light with lacquer thinner, or something similar, to insure a clean surface for the bonding of the epoxy.

For the first pass, use the epoxy immediately upon mixing it while it is soft so it will gradually flow to fill the cavity. This epoxy fill is intended to fully fill the cavity, even with the edges of the fender light base. Wait a minimum of 12 hours for this first load of epoxy to cure.

For the second pass, you are going to add just enough epoxy to the surface, such that the epoxy is slightly higher than the fender light sides. The idea is to add just enough epoxy that the fender light sides do not rest on the fender, but not enough so that it is visible when the fender light is resting on its rubber pad. For this you will mix the epoxy, but let it stand for a few minutes so that it is firm enough that it does not "flow" when applied. Wait another 12 hours for this second load of epoxy to cure.

Carefully sand the epoxy to make a smooth surface that is properly contoured to your fender. The epoxy can also be drilled in the appropriate location to pass the wire(s) from the fender into the fender light.

Fender light base with epoxy reinforcement

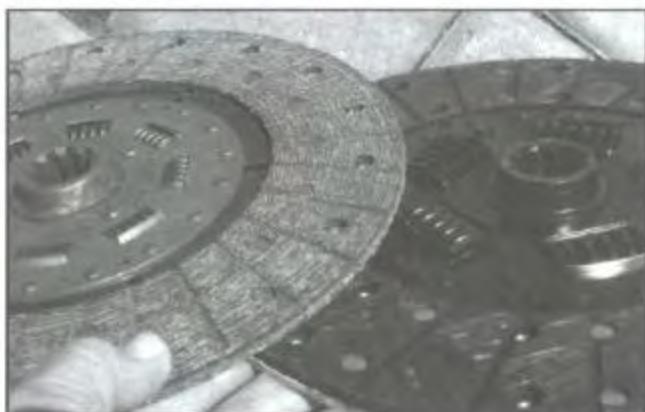


BUYING A NEW CLUTCH PLATE

TECHNICAL TIPS

Story and photos by Harry Logan #651

Be careful when buying a new clutch plate and make sure that the springs are not large enough to hit the bolts on the end of the crankshaft. A friend was having trouble installing a new clutch plate (shown at right) in his 1938 Roadmaster. Turns out the springs were hitting and the clutch plate could not be installed. Notice how the springs protrude on the right plate compared to the NOS plate on the left. Note: Big Series cars used a 11" diameter plate while Specials used a 10" plate.

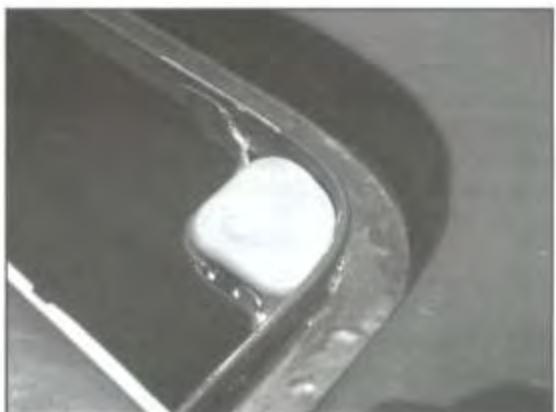


TECHNICAL TIPS

Story and Photos by Harry Logan #651

1937-1938 Rumble Seat Bumpers

Here are photos of the Lynn Steele rumble seat rubber bumpers. I found them in the Cadillac section of the Steele catalog. They also fit 1937 and 1938 Chevrolet convertibles and probably LaSalle also. The Steele part number is 70-0152-87. On the right is the bumper that was on the car with the new Steele bumper on the left. They are installed in the upper left and right corners of the rumble seat opening.





TECHNICAL TIPS



THE CORRECT CONVERTIBLE WINDOWS



Story and photos by
Harry Logan #651

I was at the Hillsborough Car Show last Sunday and saw a 1937 Cadillac Convertible Coupe with an aged but correct rear window. The same window was also used on 1937 Buicks as well as other GM cars in 1937. The 1938 window did not have the vertical bar in the center. Members who own Buick Convertible Coupes often wonder what is the correct rear window and might be interested in seeing these photos.

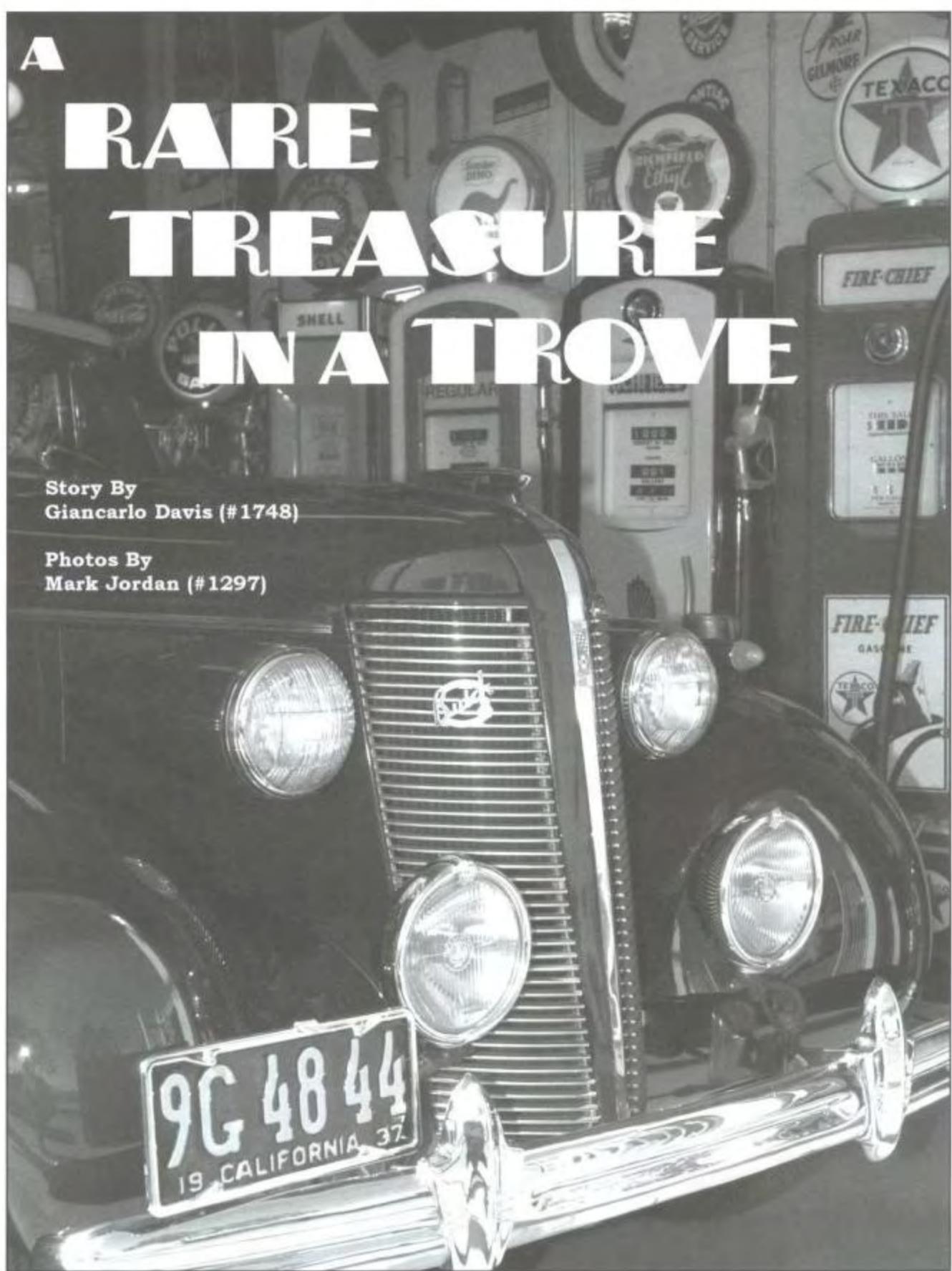


A

RARE TREASURE IN A TROVE

Story By
Giancarlo Davis (#1748)

Photos By
Mark Jordan (#1297)



T

he chances of discovering Frank Ghilgione's garage, a treasure trove of thirty pristine restored and maintained vintage automobiles that are hidden in a dull industrial area of Alameda, California, are rare indeed: a golden oyster pearl would have been easier to discover. But heavenly forces were with *Torque Tube*: our hapless editorial team accidentally came across the garage one cold, rainy afternoon. Ghilgione is not only the proud owner of the large space stuffed with cars and other pieces of vintage automobilia—he is also the owner of a rare 1937 Canadian Buick Century. That day led us to a rare find within a rare find.

The Century is a beauty. We were happy to chat with Frank, and were even more elated when we discovered that he moved the Buick from the back of his garage (and, in the process, moving several other of his rare, collectible cars) to the front before we returned the following Saturday afternoon. The Buick was now at the front of the Fords, Auburns, Chevrolets and Cord (among several others) and looked stately and proud, almost overshadowing the other cars with its own splendid beauty.

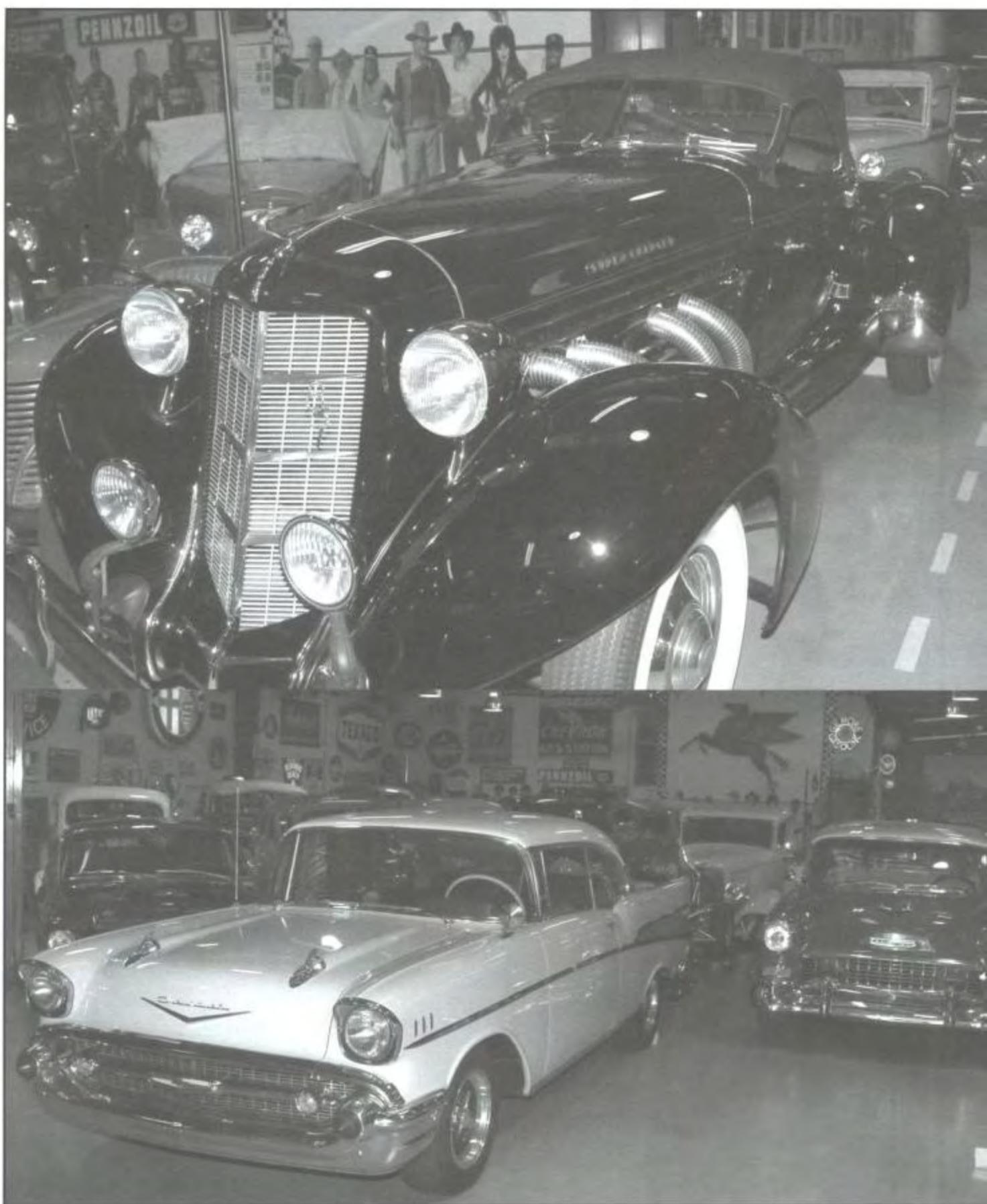
Ghilgione has had the Buick for ten years. "I bought it in Connecticut and had it shipped out

to California. It was basically in the same condition it's in now—almost perfect—except for a little mechanical work that needed to be done. As far as its rarity is concerned, I had no idea. I just knew that I wanted a 1937 Buick, and this one was in good condition. The upholstery needed to be tightened up a bit; we did a bit of chrome work. Body-wise, we straightened and repainted the right side quarter panel. Mechanically, we just needed to replace little things, like the muffler. Other than that, it was pretty much in the condition it's currently in. All the parts are original. The radio also didn't work, but I found a guy here in Alameda who did a phenomenal job in fixing it. He matched the tubes and now it works perfectly."

Ghilgione's rare Buick (as well as the other automotive gems in his expansive collection) is an object of great curiosity for many passersby, but he doesn't enter the Canadian car into car shows. "I simply don't have a lot of spare time. I spend a couple of hours here in the garage on Saturday morning, and that's about it. I get a lot of comments on it, though. Usually, the same twenty people who come in and talk about it. Everybody likes a different car here."

Unlike other cars in the collection, the Buick tends to be less of a museum piece than the other, more rare, cars parked in the garage (Two of the most impressive pieces are an Auburn and an







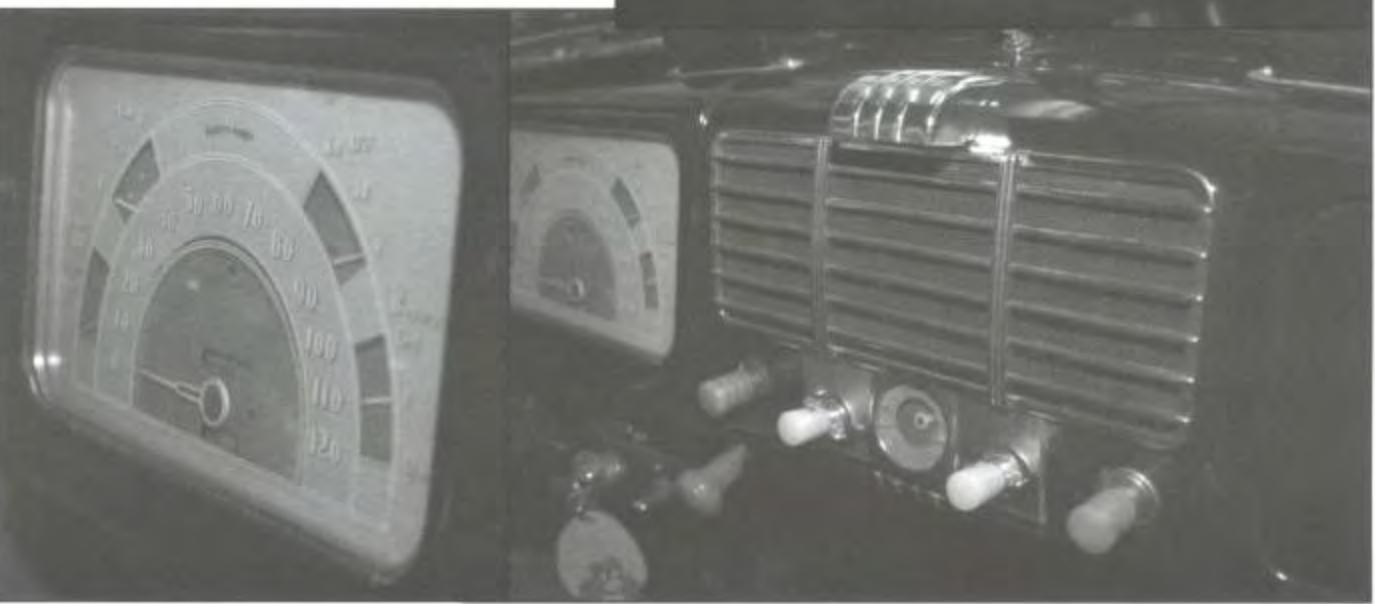


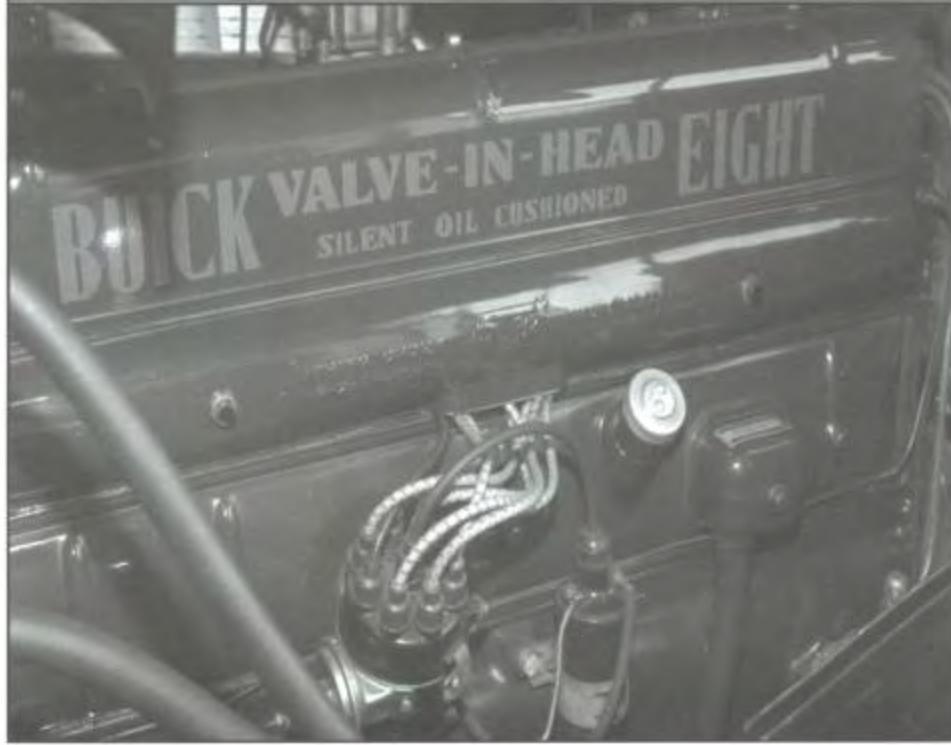
behaved car."

Buicks have always been in Ghilgione's family, and it's another reason why he has such a strong affection for the car. "I actually remember the 1937 Buicks in my high school days. My dad, grandfather, and uncle all had Buicks. I also learned how to drive on my dad's 1950 Buick. The Buick has always been in my family. The old Italian theory was 'don't buy a Cadillac . . . buy a Buick, because no one is really going to know the difference (and the Buick is cheaper). It really only is about a half-scale away from the Cadillac anyway, and it costs

immaculate 1936 Cord). Ghilgione takes the Buick out pretty frequently, and not only enjoys its comfort and ease of handling, but its reliability as well.

"It's the only car I take when I go to Tahoe. I have it towed up there, but when it's there, it runs like a top and has plenty of power. It's also one of the only cars that will fire right up after I haven't driven it in awhile. I've driven it up from here [Alameda] to Clayton, California [about 30 miles]. They're very solidly built cars. Temperature-wise, I never have an issue with it: it could go straight up a cliff if it wanted to. The car has never really had anything wrong with it. It's a very well-





less."

But Ghilgione's love transcends history: he likes the car's style over the style of other cars as well. "I pretty much like all of the car, style-wise. But then again, I like all 1930s-styles. I like the running boards, the fact that they're narrow, and the 15-inch wheels. I think that, again, in comparison to Cadillacs, they're less ornate and they're nice and subtle."

The owner of a local trucking company, Ghilgione started collecting old cars back in 1979. His garage, which now houses thirty superbly restored automobiles ranging from 1932 to 1970, once served as a customs house for his business wares. The space lacked a dock

and a ramp, which made it difficult for him to get the trucks in and out. So, after starting his collection of cars, he opted to transform the customs house into a garage. "I filled all the nooks and crannies with cars . . . much to my wife's chagrin,"

he adds with a chuckle. "We do all the mechanical stuff here. No body work."

The collection had become so large and impressive, he hired eight full-time mechanics to keep the cars in the perfect shape they're in now.





"They do a great job. Some of these cars are now worth in the hundreds of thousands of dollars." Looking at Ghilgione's amazing collection of rare autos (including the rare Canadian Buick), that's not hard to believe at all.



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'37 Grille in vg condition... \$500
'37 Grille set, needs re-chroming... \$200
'38 Grille in vg condition... \$500
'38 Front fenders, excellent condition... \$500-\$1,000 pr.
'38 Grille, left half, good condition... \$150
'38 Special hood, vg condition... \$100
'38 Special hood side panels, vg condition... \$100 set
'38 Deck lid, good condition... \$100
'37-'38 Doors, many, what do you need... \$100 ea.
'37-'38 Bumpers, vg condition... \$100 ea.
Misc. heads for both series engines... \$95
Small series engines... \$100 to \$300

Lots of '37-'38 parts. All prices, plus S & H

Lloyd Ikerd (#1612)
949-640-8200
elzfargo@msn.com

1937 Buick Special Parts For Sale
Bell housing \$25
Fan blade \$10
Fan pulley \$8
Front spindles (pair) EXC \$125
Robe rope escusions, good \$10 pair
Trunk emblem clear glass piece, \$10
NOS Wheel hubcap clips (10), \$10
Torque Tube \$100
Differential gear assembly, exc, \$125
Rear axels (pair), \$75
Front brake drums, \$25 each
Rear brake drums, \$25 each
Brake shoes (g) \$25 pair
Dash (g) \$65
Door garnish moldings (four door) \$30 each
Windshield moulding (g) \$60
Rear Window moulding (g) \$60
Radiator, nice condition, but needs minor repair, \$100
Oil pan, \$25
Lifter assembly, \$40
Brake backer plates \$20 each
Front wheel bearings (used) VG \$30 pr
Transmission, VG \$125 (will not ship)
Speedometer and Odometer gear assembly VG \$25
Fender welting (black), roll \$10
"BUICK 8" grill emblem (nice) \$25

1937
Nose grille and headlights... \$250
Right front fender (Century)... \$100
Grille... \$150
Roadmaster engine... \$600
Small engines... \$400 ea
Century hood... \$100
Special hood... \$90
Rear splash pan... \$50

1938
Left front fender (Century)... \$100
Grille... \$100
Special hood... \$90
Rear bumper... \$50
Front bumper (welded)... \$15
Rear splash pan... \$50

Alan Mattei (#700)
7306 Willow Place, Canastota, NY 13032
315-697-2814

Emblem, center strip on nose, (good) no chips \$35
"SPECIAL" louver name tag (right side only), \$10 each
Heater switch (aftermarket) taupe knob, \$8
1937/60 Inside door chrome strips (4) \$25

1937 LITERATURE
Original Advertisements (16) in binder, asking \$50 OBO
Radio manual, copy, in binder, \$10
Large binder with copies of, paint chips, radio
manual, Fisher body manual, Parts catalog, Transmission
and Rearend manual, FILKO ignition parts manual, Tons
of useful information with many illustrations.
GREAT FOR THE GARAGE, printed on reinforced 3 hole
paper in an expensive heavy duty expandable binder.
Asking \$100 or best offer.

AUTOMOBILIA
1937 BUICK WALNUT PLAQUE, with a 3 3/4" cast bronze
emblem (excellent condition) \$125
LAST CALL (WILL NOT SHIP)
BLOCK \$100 HEAD \$75 CRANK \$50

I have lots of odds & ends left, call with needs & wants

Jerry Root (#422)
71 South Pollard Drive, Fulton, NY 13069
315-598-2319, buickboy@twcny.rr.com

1937-38 Buick Parts

'37 Rear bumper, 40-60 series, used, fair...\$40.00
 '37 Bumpers gaurds, used, fair...\$15.00ea.
 '37 Bumper gaurds used, need grinding on repair, replat-ing...\$5.00ea.
 '37 Hood, 4 sections 40 series used, good...\$90.00
 '37 Lamp, rear center, trunk back body, used, fair to good...\$25.00
 '37 Lamp, rear center, slope back body, used, fair...\$25.00
 '37 Glove compartment door, with clock, no lock, used needs refinishing...\$20.00
 '37 Radio dial, numerals on glass, used, good...\$10.00
 '37 Steering wheel, banjo style, used, core for remold-ing...\$30.00
 '37 Center nose badge, used, fair...\$10.00
 '37 Spare tire clamp for models 81, 81F without side-mounts, used, good...\$15.00
 '37 Generator brush plate assembly, P/N 1866417, NOS...\$15.00
 '37 40-60 Series taillight pads, new, fine repos by Steele...\$21.00pr.
 '37 Parking light fender lamp pads, new, fine repos by Steele...\$21.00pr.
 '37/'38 Series 40 and 60 Brake and clutch pedal pads, new, fine repos by Steele...\$12.00pr.
 '37/'38 Headlamp reflectors, used, good...\$65.00pr.
 '37/'38 Headlight rim, used, fair to good...\$25.00ea.
 '37/'38 Dome lamp rim, lens broken, used, fair...\$6.00
 '37/'38 Distributor vacuum control, P/N 681-H, NOS...\$20.00
 '37/'38 Distributor contact point set, P/N 1871870, NOS, 10 in package...\$25.00
 '37/'38 Distributor breaker plate, P/N 1865968, NOS...\$20.00
 '38 Series 40 & 60 differential bevel side gear, Gr.5.528, P/N 1302327, NOS...\$35.00
 '38 Oil pump, series 40, used for core...\$10.00
 Shipping cost will be quoted extra.

H.J. Glass (#111)
 7 Long Leaf Cir.
 Fairhope, AL 36532
 251-990-6050

1937

Front lower motor mount...\$15
 40 ser. 2nd speed gear...\$45
 40 ser. main drive gear...\$45
 40, 60 ser. differential bevel side gears...\$20 ea.
 40 ser. steering gear pitman arm...\$30

1938

40 ser. clutch release rod adjustment nut...\$4
 License plate lamp lens...\$4
 80, 90 ser. pitman arm...\$30
 Right taillight door, used...\$10
 80, 90 differential side gear bevel thrust washers...\$3 ea.
 40, 60 ser. outer tie rod ends, n.o.r.s...\$20 ea.

1937 to 1938

40, 60 left front shock absorber...\$125
 40, transmission, 2nd & 3rd speed shifter yoke...\$15
 40, transmission rev. idler gear thrust washers...\$3 ea.

1937 Roadmaster sedan passenger side front fender-nice shape, one minor crease...\$125
 One gas cap, nice condition...\$5
 Bumper guard needs rechrome...\$7
 Voltage regular n.o.r.s. criterion brand, original box...\$35
 Please add 15% for shipping

Lewis Cohen (#584)
 58 North Racebrook Rd.
 Woodbridge, CT 06525,
 203-397-8723

1937 Buick Special Coupe
 1 pr. running boards, with new rubber covers...\$250 pair
 Rubber covers installed on the wrong sides, but easily re-installed properly.
 1 pr. headlights with 12v halogen bulbs...\$125 pr.
 Original windshield wiper vacuum motor...\$35
 Original 4.40/1 rear axle ring and pinion gear with drive shaft...\$200
 Original 3-speed transmission...\$250
 Original dome light...\$75
 Rebuilt Stromberg carburetor...\$125
 Original air cleaner, rebuilt and repainted ready for de-cals...\$550
 6.50 X 16" Firestone bias-ply tires, in very good condition; with tubes.
 New set of 4 WWW, with original black spare...\$300
 Original 6-volt starter motor...\$200
 Original 6-volt generator...\$125
 Original split front bench seat base, with new beige cloth and vinyl upholstery: seat backs already gone...\$125
 1 Pr. original door handles, one locking...\$75
 Original trunk handle...\$50
Jerry Maak (#1812)
 343 Eastern St., New Haven, CT 06513
aurhaus@yahoo.com
 Phone: 203-468-9880 Cell: 203-687-7649

Two 16-inch wheels for a 1938 Buick Special. Will accept best offer. Good condition. Need to be refinished.

Rick Yost (#1704)
 805-374-9093, rednyld@aol.com

40, 60 ser. pinion bearing locknut...\$3
 40 ser. intake, exhaust valves, specify...\$8 ea.
 Rear-axle pinion-bearing lock screw...\$1
 40, 60 pitman shaft bushings...\$15 pair
 Headlight dimmer switch, n.o.r.s...\$10
 80 ser. outer front wheel bearing...\$25 ea.
 40, 60, inter. steering arm bearing kit, n.o.r.s...\$30 ea.
 Outside door handles, used, 3", 3 5/16" shafts, 4 15/16, 5 1/4, 5 1/2" tall...\$10 ea.
 40, 60 ser. left steering knuckle with bushings...\$40
 40, 60 lower outer pivot pin kit, n.o.r.s...\$25 ea.
 Ignition point set...\$7 ea.
 parts n.o.s. except noted. Add 15%, \$3.50 min. postage

Bob Graves (#1136)
 21 Ferry Rd. Salisbury, MA 01952
 978-463-0715 evenings

For Trade

Intake manifold for '37/'38 320 engine, sandblasted and painted with POR 15 manifold paint. To trade for intake to fit a '49 320 engine.

For Sale

Later 320 engine water pump cores suitable for rebuilding. Part numbers 339915-6 and 1336756-1C.

\$1 each.

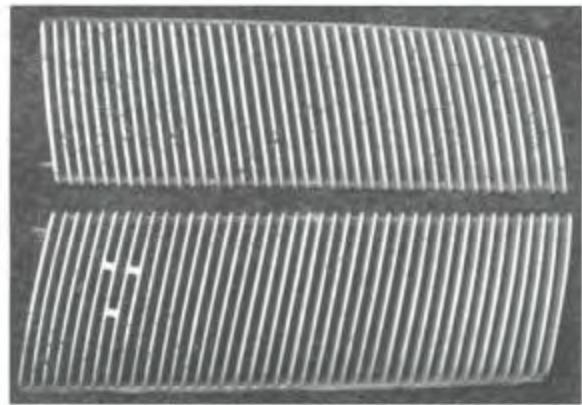
Front half of "flywheel cover" plate to fit a Dynaflow to a 320 engine. Part number 1328296. \$10.

Malcolm Fischer (#1746) Alberta, Canada
sandguys@telus.net
403-742-1663

1937 aluminum grille. Brand new....\$300

Ken Hale
510 656-4523

Fremont, CA, kenhale001@comcast.net



1937 Buick Roadmaster 81 4-dr Sedan

Fenders, nose sheet metal, grille, doors, trunk lid, frame, etc.
Call or e-mail me with needs.

Jim Garrett (#1826)
207-929-8035 or patjim@sacoriver.net

Two 16-inch wheels for a 1938 Buick Special. Will accept best offer. Good condition. Needs to be refinished.

Rick Yost (#1704)
805-374-9093
rednyld@aol.com

1938 radio, glass has crack, these are getting hard to find....\$400 plus shipping.

Don Helmrich [1908]
321 676-4123
donh25@juno.com

Four 1938 Hirsch reproduction hubcaps for GM built car. Never been installed. All four: \$280 plus shipping.

Jack Remesoff (#562)
6307-45 Ave.
Camrose AB, CANADA T4V OC3
780-672-4722

1940

Parts car 9 (Century no motor or transmission)...\$500

1948

Complete Roadmaster...\$500, Steering column...\$25

1951

Steering column with wheel...\$75

1952

Dynaflow transmission...\$50

Alan Mattei (#700)

7306 Willow Place, Canastota, NY 13032
315-697-2814

Complete differential for '38 special.
this from a low mileage (37,000) car. \$100
plus shipping.

Pat Jacobs (#1308)
360-568-3212, patlyndell@yahoo.com

Two pieces of rear-door window glass to fit 1938 to 41.
They're **tinted** glass; I'll send them to anyone who can use
them for no more than the cost of shipping.

Paul DeLucchi (#1246)
pauldelucchi@earthlink.net

1938 ALL SERIES 40 PARTS

Complete engine—less intake manifolds. Includes
starter,
gen. carb, air cleaner, etc.—ran but smoked when
removed...\$350 Transmission...\$125
ANY REASONABLE OFFER PLEASE!
I DONT WANT TO SCRAP IT!

Dale Crist (#840)
303 N. Athletic St., White Pigeon, MI 49099
269-483-9175

4 '38 hubcaps vg - m, no cracks...\$85
60-80 Series dry air cleaner unit...\$150

60-80 Series closed car radio and speaker...\$175

'38 tail lite complete...\$110

'38 dash radio grille...\$50

'38 grille...\$200

'38 hood ornament...\$60

'38 inside & outside door handles & knobs, some
with keys...\$20 to \$70

'37 clock complete but faded...\$40

Art Fensod (#1758)
357 Country Lane
Loudon, TN 37774
865-408-0525

'37 auxillary speaker...\$75
 '37 master heater...\$75
 '37 LS Stromberg AA2...\$250
 '38 Master heater with defroster...\$100
 '38 Special rims exc. Cond...\$75 ea.
 '38 Special 3.60 rear end complete...\$600
 '37 & '38 parts:
 LS air cleaner...\$125
 Limited dome light...\$75
 Coupe opera seats complete...\$600
 LS sparkplug cover...\$100
 Sidemount lock...\$25

LS oil bath air cleaner...\$125
 SS & LS generators...\$75 to \$100
 Horn rings...\$100
 Century running board cores...\$300
 Special running board cores...\$200
 Call w/your needs, always parting '37 & '38s.

Dave Tacheny (#997)
 11949 Oregon Ave. N.
 Champlin, MN 55316
 763-427-3460

1937 - 1938 Parts for Sale

'37 repro, silkscreen glass speedometer, radio and clock...
 \$100
 New '38 Century Stainless "hood hinge"...\$175
 S/M badges Buick...\$65
 '38 Century hood sides, left and right...\$150
 '38 Century hood, left side only...\$75
 '38 Century hood sides, left and right...\$150
 '38 Century hood side, right only...\$50
 '38 Century hood side vent, right and left...\$75
 '38 Century hood stainless...\$75
 '38 Century hood lock mechanism...\$50
 Large series fuel pump rebuilt...\$75
 '38 Special hood, right and left, with hinge...\$150

'38 Special hood sides, right and left...\$150
 '38 Special hood stainless, left and right...\$120
 '38 Special tail lights, pair...\$75
 '38 Special front suspension, hub to hub...\$200
 Large series dual carb manifold...\$250
 "Y" pipe to create dual exhaust...\$50
 '38 repro grill, left and right, cast aluminum...\$250
 '38 Special radiator...\$75
 '38 heater...\$50

Shipping will be added to all purchased parts and is not included in price above.

Rod Phillips (#1644)
 913-481-6763

'38 N.O.S. right grille half, not plated, excellent...\$190
 '37 & '38 N.O.S. grille bug screen w/Buick logo & hardware...\$180
 '38 battery lock with key, excellent...\$54
 '37 & '38 sidemount lock with key...\$65
 '37 & '38 N.O.S. sideview mirror w/thermometer (non-convertible)...\$225
 '37 & '38 N.O.S. Delco coil...\$95
 '37 & '38 N.O.S. Delco voltage reg...\$175
 '37 & '38 guide headlight lenses (pair left & right)...\$255
 '37 & '38 ignition switch box, good, used...\$40
 '37 bronze ashtray with front-end casting of car N.O.S....\$215
 '37 three vibrant color posters 21" / 16", full color, rare & frameable...\$50
 '37 & '38 Buick Safety Legion license tag metal badge, original...\$75
 '37 & '38 N.O.S. gas tank door lock, in box with key and instructions...\$83
 '38 Clock deletion plate, engraved with Buick 8...\$75
 '37 & '38 Fulton interior rearview mirror green flip down lense...\$57

'38 to '40 Series 80/90 master cyl. kit Delco, N.O.S., rare...\$70
 '38 instrument cluster original, exc., complete...\$300
 '38 radio with knobs, not tested but complete and excellent...\$270
 '37 & '38 Red Crown air stem caps, set of four...\$12
 '37 & '38 cowl mount antenna, N.O.S., all mounting and leads, exc....\$167
 '38 *Buick Announcement Magazine*, full color foldout, all models...\$185
 '37 & '38 Convertible rearview mirror...\$260
 '37 & '38 instrument light and map light switch original...\$55
 '37 & '38 16-inch series 80/90 wheels, exc....\$400, set of four

Vince Truant (#1220)
 1537 Charmuth Road
 Lutherville, MD 21093
 410-321-1938

Piston rings for a 40 series, 1938-style pistons. They are standard size. \$80.

William Schwantes (#878)
 9973 County Valley, Glen Haven, WI 53810
 608-794-2406

Intake & exhaust manifold
 3.90 Axle

One headlight bucket
 Misc. stainless. Call for prices

Frank Cwikla (#1111)
 1909 Goodin Rd., Friendship, WI 53934
 608-339-6297

PARTS WANTED

1937 horn ring for steering wheel

David Watson (#1863)
250-753-2997

Hubcaps for 1937 McLaughlin Buick

Kim Cousineau (#1884)
kcousine@iaw.com
905-892-3473

1938 Buick Factory Accessories Catalog

Dave Gelinas (#1078)

1131 Hayward St. Manchester, NH 03103
dkg@worldpath.net 603-668-5587

1937 Buick Special Series 40
Front license plate holder/bracket
1 pair front fender parking light glass or
plastic lenses
Radio working or non-working
John Luersen (#1788)
24 Morris Rd.
Prospect, CT 06712
johnluersen@sbcglobal.net

1937-38 Buick
rumble seat complete, or hardware
to build one, including deck lid, seat brackets, etc.
Lloyd Ikerds (#1612)
949-640-8200

1937 or 1938 Special/Century Convertible
4-door Sedan
Front seat in good sound condition to reupholster
2 removable door posts for top, solid rechromed
2 front window frames, solid rechromed
4-door garnish moldings, solid rechromed
Top prices paid for good solid parts. No junk
John Sauers, 620 Wright Loop
Williamstown, NJ 08094

2005 - 1937/38 Buick Club calendar.
Jerry Barton (#281) (360)825-5230 &
Jerry Maack (#1616) (908) 389-1464

1938 Buick Roadmaster
Gas tank straps with bolts
Tie rod 1298347 Group 6.230
Lower Control Arm Shaft 1291540
Group 6.170
NOS exhaust solid bracket front
most

NOS exhaust brackets rear most
Right headlight housing

Brian DePouli (#1914)
Charlotte, NC
704-236-8423
Brian.depouli@att.net

Complete rebuildable 1937 special
engine and transmission.
Frank Freda (#1838)
frankfreda@cox.net
805-969-7500

2 '38 parking light lens
2 dished plates that hold wheel to
sidemount bracket

J E Mullenix #436
(616)945-5807
jmullenixsr@sbcglobal.net

Windshield wiper transmissions for a
1938 90L
Jan Burry (#1478)
315 Manitou St. Northfield,
MN 55057 507-663-0931

Two sidemount thread covers stainless steel trim pieces for a 1938 Buick Special. I believe the 1936 thru 1938 Buicks trim pieces will work. Or has anyone figured out a substitute?

Herbert Sutton (#4)
3760 Cleveland Hill Rd., Roseburg, OR 97470
541-672-8414 lrose@mcsi.net

Rumble seat fender step plate for
1938 Buick Century 66C. Any condition
and any reasonable price.
Jeffrey Hery (#1464)
12 Wright St., Parkesburg, PA
610-857-1458

1936 or 1937 Roadmaster pinion gear, part no. 1287780, or ring
Pinion gear set, ring gear, part no. 1287781.
1936 Century ring and pinion gear set, pinion gear part no. 1294231,
ring gear part no. 1294230.

Tom Greer (#1378)
499 Hubbardston Road
Princeton, MA 01541 978-464-7725
wtgreer@ekmail.com

DELCO-REMY Distributor #
1110801 (fits 1938, 1939 & newer)
prefer N.O.S. or a rebuilt with the
Delco tag still on it
-DELCO-REMY ignition coil #526-B
(fits 1930s Buick through 1939)
prefer N.O.S. or really good condition
used
John Koutre (#593)
2331 Rainbow Dr., Plover, WI
54467
715-341-6188

1937 Roadmaster rear bumper
gravel guard and running
board's rear gravel guards.
John Welby (#1497)
1665 Shipman Rd.
Oxford, MI 48371
248-628-8787

Air cleaner regular-series 40-1937
Voltage regulator: Delco-REMY 5807
Carb: Stromberg AA-1
Vacuum starter: Delco REMY 1607
Joe Pruce (#1721)
19341 Malvern Ave., Rocky River OH
44116 440-331-7427
jpruce@hotmail.com

CARS FOR SALE



Model 46. It officially is a 46C, but before it came from CA the owners took out the shelf and put in the back seat. It is a business coupe that looks like a sport coupe. When it came to IN all the glass was replaced. It was repainted (original color) sometime before it was in the motion picture "A League of Their Own," shot here in the rural Evansville, IN area. We were told the engine is original, but I have not yet checked the numbers. It has the 2 bbl. Stromberg Aerotype as it should. Except for the addition of the back seat, interior is nice and original. GM seatbelts added. Has options of steering wheel with trim and the glove box clock. The dash was expertly refinished beautifully by local artist. Wide whites are by Lester. Original hub caps. Some chrome is great, some could use a refinish. New fuel cell by local shop in Mt. Vernon known for their good work with antique cars. We drive to all the local shows. It runs great and rides smooth. Engine is 248 Dynaflash and has plenty of power. Before we owned it someone added an electric fan to help with city traffic on hot days.



\$15,000 OBO
Matt Kemmelting
428 Kennedy Drive
Mt. Vernon, IN
812-589-1838

1938 Buick 40C, beautiful 4 door convertible. New top and paint. Runs great!
\$44,00 OBO #11/2 Car

Alan Mattei (#700)
7306 Willow Place
Canastola, NY 13032
315-697-2814

CARS WANTED

1937/1938 Buick coupe convertible with sidemounts. Car should be in good running order and in 2 or 2+ condition

Jack Granowitz (#1944)
201 410 3326
JMG10@columbia.edu

1937 or 1938 Buick 2 door coupe, Special or Century. Condition 4 or better.

Tom Halfpenny (#1286),
Woodbury, MN
651-738-0838
tmhalfpenny@msn.com

1937 or '38 Special or Century Convertible with sidemounts in original condition. Does not need to be in show condition, I would like an original driver car.

Richard Beckley (#1945)
717-938-4975
dbeckley@epix.net

1937 or '38 Century parts car or coupe or 2-door sedan

Walt Lopic (#999) 724-843-8206

THE 1937-1938 BUICK CLUB STORE



LONG SLEEVE T-SHIRT
\$24.99



WHITE T-SHIRT \$18.99



GOLF SHIRT
\$22.99



STICKER (oval) \$4.99



WALL CLOCK \$18.99

THESE ITEMS NOW AVAILABLE ONLINE @
<http://www.cafeshops.com/torquetube>

If you don't have internet access
you can send your order directly to:

1937 - 1938 BUICK CLUB
P.O. BOX 21000
OAKLAND, CA 94620

Please add 15% for shipping and handling
Please include shirt size



LARGE MUG
\$15.99



MUG
\$14.99



STAINLESS STEEL
TRAVEL MUG
\$18.99



2006 1937-1938
WALL CALENDAR
\$19.99



BASEBALL CAP
\$18.99
Black Only



MOUSE PAD
\$14.99

Welcome to our New Members

James Blossom #1941
1304 Boone St.
Troy, MO 63379
1938-80-8

Jack Granowitz #1944
463 Paul Ave.
Allendale, NJ 07401-1861

Irving Bradstreet #1942
24 Bradstreet Dr.
Wales, MA 04280

Richard Beckley Jr. #1945
P.O. Box 128
Lewisberry, PA 17339-0128
37-80C-8

Jeff B. McCain #1943
917 Benjamin Parkway
Greensboro, NC 27408
38-46-5

Alan Booth #1946
5905 Taylor Valley Ave.
P.O. Box 34045
Las Vegas, NV 89133



NOW AVAILABLE! INSIDE FRONT and BACK PAGE FULL COLOR ADS

For competitive rates and information, please contact the editor Mark Jordan.

TORQUE TUBE
P.O. BOX 21000
OAKLAND, CA 94620

Mark Jordan: 415-203-4180, torquetube3738@yahoo.com, www.torquetube.com

ATTENTION ROADMASTER AND LIMITED OWNERS

I am looking into reproducing the "Chromium Wheel Disc" (covers the area between the beauty ring and the hubcap) listed in the 1937 accessory catalog as p/n # 980547. Although they were only offered in 1936 and 1937 they would look great on a '38 as well.

Reproduction will be in 18 gage 304 stainless steel. Please let me know if you are interested as this will be a one-time only run due to the limited application. Please note: they will not fit Century or Special rims.

Robert Ward
25 York St.
RR #1, Sutton West, ON
Canada, L0E 1R0
the37limo@sympatico.ca



1937-38 BUICK PARTS

**A VARIETY OF
NOS, USED &
REPRODUCTION PARTS**

*Gauges, Handles, Mirrors
Interior Detail Items & Plastics,
Belt Molding, Fuel Sending Units,
Some Mechanicals PLUS MUCH MORE!*

**FOR A FREE LIST, CONTACT:
AL BELL BUICK PARTS**

**4982 Hubner Circle
Sarasota, FL 34241
Phone: 941-927-3588
Fax: 941-926-8224
Email: buickpts@comcast.com**

SPECIALIZING IN BUICK PARTS 1919-1958

1937-1938 Buick "HOOD HINGES"

All Stainless Steel Reproduction of the Original Trim
Orders Cut To Length To Fit Your Model and Year

\$195
plus shipping and tax (CA)

Bob's Specialty Parts
9282 Sungold Way, Sacramento, CA 95826
(916) 362-2597

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Detailed Restoration on
All Vintage Buicks

Specializing in 37 - 40 Buicks

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Springfield, Illinois 62703
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davelewisrestoration@msn.com

WORLD'S BEST MARKETPLACE FOR VINTAGE CARS



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MYSTERYCARS

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MOTORTREND
CARANDDRIVE
CARANDDRIVE.COM



1904-1965 BUICK RESEARCH SERVICE

More information and detail about your specific 1937 or 1938 Buick year, model and serial number than you can get from any other source. SATISFACTION GUARANTEED.

Complete Research Package \$50.00, \$60.00 if shipped outside the USA.

Additional information available at <http://www.OldBuickArchive.com>. Also check out 1937 and 1938 in the "Valve-In-Head Archive" at the web site for a listing of reference material that can be photocopied for research purposes.

1937 BUICK HIGH QUALITY VIDEO

1937 Buick video made from the only known copy of a 16mm black & white sound film produced by Buick Motor Division in 1937. The tapes have been made by a professional video dubbing studio. They are very high quality and are produced from a digitally mastered original.

Video runs approximately 32 minutes and contains a number of segments showing the construction of the 1937 Buick and Buick's place in American society. Historically significant item. SATISFACTION GUARANTEED. \$39.95 including shipping.



TERRY DUNHAM
AUTOMOTIVE RESEARCH SERVICES



P.O. Box 4057, Apopka, Florida 32704-4057
e-mail: Buickohv@aol.com • web site: www.OldBuickArchive.com

1937-38 BUICK

DOOR WEATHERSEAL-SPONGE

Glue-in.....	DW-378.....	\$2.10 ft.
Clip-in.....	DW-80.....	\$3.75 ft.
Clips.....	WC-80.....	\$0.90 ea.

DOOR BOTTOM SEAL

Clip Type.....	DW-369.....	\$3.75 ft.
----------------	-------------	------------

TRUNK SEAL-SEDANS. 1/2 Wide;

Ser. 80-90.....	TW-371.....	\$46.00
Sedans. 3/4 Wide:		
Ser. 40-60.....	TW-371S.....	\$46.00

TRUNK SEAL For COUPES. 5/8 x 1

Sponge.....	TL-369.....	\$2.25 ft.
1/2 x1 x16	TL-1129.....	\$49.50

CLUTCH & BRAKE PEDALS Ser. 40-60

Black.....	CB-343.....	\$7.25ea.
Brown.....	CB-343BN.....	\$7.50ea.

PEDAL FLOOR SEALS; All Models

FS-375.....	\$13.75 pr.
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1937 ONLY! Accelerator PEDAL Ser.40-60

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FRONT END PARTS For 40-60 Series;

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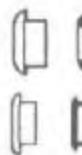
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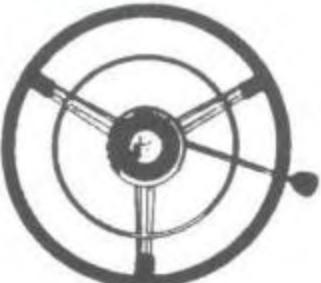
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